



Elm/Env	Element Description	Total Qty	Units	State 1	State 2	State 3	State 4
31/2	<b>Timber Deck</b>	395	sq.ft	0	395	0	0
	The timber deck is 2x6-inch timbers on edge, covered with a gravel wearing surface.						
	The gravel is well-packed and in good condition. The timber ends have minor splits. The top of the deck is punky, per hammer sounding.						
1140/2	<b>Decay/Section Loss</b>	369	sq.ft	0	369	0	0
	<i>The top of the deck is punky, per hammer sounding.</i>						
1170/2	<b>Split/Delamination (Timber)</b>	26	sq.ft	0	26	0	0
	<i>The timber ends have minor splits.</i>						
111/2	<b>Timber Open Girder/Beam</b>	178	ft	178	0	0	0
	The pressure treated 19.5 x 8-inch timber beams are numbered from west to east.						
	The timber has minor water staining from deck leakage. No rot was noted from hammer sounding; 2023 Resistance Drilling found rot in Beam 1 (CS1).						
216/2	<b>Timber Abutment</b>	80	ft	16	0	6	58
	The integral lapped jointed pressure treated timber abutments and wingwalls are made with 10 x 10-inch timbers. Each abutment has (11) deadman timbers placed for vertical support; 5/8-inch rebar is nailed through to lock the joints in place. No other rebar holds the abutments together. Each wingwall contains (6) deadman timbers.						
	The abutments are in poor condition and the south abutment is actively failing, due to rot, crushed timber, and bowing from back pressure. The top of the south abutment has settled by 1.5-inch.						
	(CS4) South abutment: 2 timber from the top has 2-inch-bending over the member with a 2-foot-long split.						
	(CS4) South abutment: 3rd timber from the top is fully rotten and has crushing and bulging with splits; it has collapsed by 1-inch.						
	(CS4) South abutment: 4th timber from the top is fully rotten and has a 1.5-inch offset.						
	(CS4) South abutment, 4th timber from top: 2023 Resistance drilling found rot up to 9-inch-wide.						
	(CS4) North abutment, 6th timber from top: 2023 Resistance drilling found rot up to 8-inch-wide.						
	(CS4) (11) rebar pin connections have failed due to rot or back pressure.						
	(CS4) South abutment: bottom timber has a 2-inch offset.						
	(CS4) North abutment: bottom timber has a 1.5-inch offset.						
	(CS4) The wingwalls have all been heavily effected by the wildfire and are burnt to 50% structurally ineffective. The ends are particularly burnt and the timbers are cracked in half.						
	(CS3) SW wingwall: structural failure from the burn with a 5-degree tilt.						
1020/2	<b>Connection</b>	11	ft	0	0	0	11
	<i>(CS4) (11) rebar pin connections have failed due to rot or back pressure.</i>						
	<i>(CS4) South abutment: bottom timber has a 2-inch offset.</i>						
	<i>(CS4) North abutment: bottom timber has a 1.5-inch offset.</i>						



## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02

1140/2 **Decay/Section Loss** 47 ft 0 0 0 47

*The abutments are in poor condition and the south abutment is actively failing, due to rot, crushed timber, and sliding from back pressure.*

*(CS4) South abutment: 3rd timber from the top is fully rotten and has crushing and bulging with splits; it has collapsed by 1-inch.*

*(CS4) South abutment: 4th timber from the top is fully rotten and has a 1.5-inch offset.*

*(CS4) South abutment, 4th timber from top: 2023 Resistance drilling found rot up to 9-inch-wide.*

*(CS4) North abutment, 6th timber from top: 2023 Resistance drilling found rot up to 8-inch-wide.*

*(CS4) The wingwalls have all been heavily effected by the wildfire and are burnt to 50% structurally ineffective. The ends are particularly burnt and the timbers are cracked in half.*

*(CS3) SW wingwall: structural failure from the burn with a 5-degree tilt.*

4000/2 **Settlement** 6 ft 0 0 6 0

*(CS3) SW wingwall: structural failure from the burn with a 5-degree tilt.*

7000/2 **Damage** 26 ft 0 0 6 20

*(CS4) The wingwalls have all been heavily effected by the wildfire and are burnt to the point of being 50% structurally ineffective. The ends are particularly burnt and the timbers are cracked in half.*

*(CS3) SW wingwall: structural failure from the burn with a 5-degree tilt.*

330/2 **Metal Bridge Railing** 52 ft 52 0 0 0

The bridge rail is w-rail on timber posts on 8x8-inch timber curbs.

The railing is in good condition. The w-rail coating is faded but still effective. The curbs are in fair condition with abrasion along the sides; the ends have impact damage from snow plows.

515/2 **Steel Protective Coating** 130 sq.ft 130 0 0 0

*The w-rail coating is faded but still effective.*



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### Additional Information

FILL: Average fill height is 1.5-inch; the gravel height along the curbs is up to 5-inch.

ROADWAY APPROACHES: South approach alignment is straight; north approach alignment has a slight curve. Both approaches are smooth and well-graveled.

EMBANKMENT: The embankment has recently been burned and is heavily impacted by the fire and by the heavy machinery used during the fire. Only young grasses are hold the soil in place; there is potential for heavy erosion during the spring runoff.

CHANNEL: The well-established channel is lined with medium to large rocks. The upstream channel is filled with small to large trees and woody debris from the fire. Channel cross section done in 2023.

SIDEWALKS/CURBS: Rail attached to curb.

DRAINS: The gravel buildup blocks the 4.5-inch gaps under the curbs, inhibiting drainage.

SIGNS: Hazard markers are set at three bridge corners; the NW marker is missing; the SE marker is badly faded. Both approaches have new [3 TON] load limit signs.

GUARDRAIL: None.

UTILITIES: None.

NOTES: The exact construction date is unknown, but a similar bridge in the area built in 1965; Idaho County notified ITD about the bridge and added it to the system in 2023. The substructure is rated a 3 due the south abutment settlement; the county will be going out to the bridge every 3-months to take photos of the south abutment.

SCOUR REVIEW: N/A

INSPECTION FREQUENCY: The bridge is on a 12-month frequency due to the abutment condition.

WORK ACCOMPLISHED: N/A

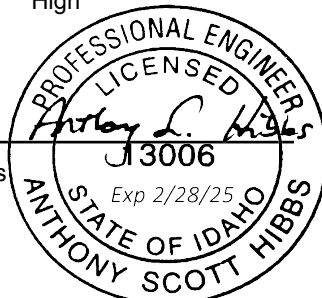
LOAD RATING: Request initial analysis. The analysis should take into account that the bridge carries heavy machinery on a regular basis.

### Maintenance Recommendations

Recommendation	Priority	Suggested Work Assignment
Repair/replace the abutments.	High	Local Agency

Inspector's Signature: \_\_\_\_\_

Inspector Number and Name: 146 - Scott Hibbs, Extreme Access



10/19/2023

Original Signed By: Anthony Scott Hibbs  
Date Original Signed: 11/28/2023

Original is stored on an Idaho Transportation Department Server, in care of the Bridge Section Database Manager



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Xref Structure Name:		District:	02

### IDENTIFICATION

(1)State: 16 Idaho  
(2)District: District 2  
(3)County: 049 Idaho  
(4)Place Code: Not within City/Town  
(5)Inventory Route: 140000000  
(7)Facility Carried: CROOKED RIVER ROAD  
(11)Milepoint: 16.151374 Agency Milepost: 016.151  
(12)Base Hwy Network: Not on Base Network  
(13a)LRS Inventory Route:  
(13b)LRS Sub Route:  
(16)Latitude: 45° 41' 47.8"  
(17)Longitude: 115° 32' 49.6"  
(98)Border Bridge Code:  
(99)Border Bridge ID:  
Segment Code: 001805  
Segment Under Rte:  
Segment Other Rte:  
Drawing Number: 18361  
Project Key Number:  
Inspection Area: 921  
MPO: N/A

### CLASSIFICATION

(112)NBIS Length: Long Enough  
(104)Highway System: 0 Not on NHS  
(26)Functional Class: 09 Rural Local  
(100)Defense Highway: 0 Not a STRAHNET hwy  
(101)Parallel Structure: No || bridge exists  
(102)Direction of Traffic: 2 2-way traffic  
(103)Temporary Structure:  
(105)Federal Lands Highway: 0 N/A (NBI)  
(110)Design Natl Network: 0 Not part of natl netwo  
(20)Toll Facility: 3 On free road  
(21)Custodian: County Hwy Agency  
(22)Owner: County Hwy Agency  
(37)Historical Significance: 4 Hist sign not determin

### GEOMETRIC DATA

(48)Maximum Span Length: 25.0 ft  
(49)Structure Length: 26 ft  
Total Length: 26 ft  
(50a)Curb/Sidewalk Width Lt: 0.7 ft  
(50b)Curb/Sidewalk Width Rt: 0.7 ft  
(51)Width Curb to Curb: 14.2 ft  
(52)Width Out to Out: 15.5 ft  
(32)App Roadway Width: 17 ft  
(33)Median: 0 No median  
(34)Skew: 0°  
(35)Structure Flared: 0 No flare  
(10)Vertical Clearance: 99.99 ft  
(47)Total Horiz Clearance: 14.2 ft  
(53)Min Vert Clr Over Deck: 99.99 ft  
(54a)Min Vert Underclr Ref: N Feature not hwy or RR  
(54b)Min Vert Underclr: 0.00 ft  
(55a)Min Lat Underclr Ref Rt: N Feature not hwy or RR  
(55b)Min Lat Underclr Rt: 0.0 ft  
(56)Min Lat Underclr Lt: 0.0 ft

### STRUCTURE TYPE AND MATERIALS

(43a/b)Main Span Material/Design:  
7 Wood or Timber 2 Stringer/Girder  
(44a/b)Approach Span Material/Design:  
(45)No. of Spans Main Unit: 1  
(46)No. of Approach Spans: 0  
(107)Deck Type: 8 Wood or Timber  
(108a)Wearing Surface: 8 Gravel  
(108b)Membrane: 0 None  
(108c)Deck Protection: None

### Deck Applications

### LRS

Route ID: 01805AOH000  
Measure: 16.15137494  
Route ID Under Rte:  
Measure Under Rte:  
Route ID 2nd Rte Under:  
Measure 2nd Rte Under:



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Xref Structure Name:		District:	02

### LOAD RATING

(31)Design Load: 5 MS 18 (HS 20)  
(64)Operating Rating: 60 tons / HS33.3  
(66)Inventory Rating: 36 tons / HS20.0  
(70)Posting: 0 >39.9% below  
(41)Posting Status: P Posted for load

### CONDITION

(58)Deck: 6 Satisfactory  
(59)Superstructure: 6 Satisfactory  
(60)Substructure: 3 Serious  
(61)Channel/Protection: 5 Bank Prot Eroded  
(62)Culvert: N N/A (NBI)

### AGE AND SERVICE

(27)Year Built: 1965  
(106)Year Reconstructed:  
(42a)Type of Service On: 1 Highway  
(42b)Type of Service Under: 5 Waterway  
(28a)Lanes On: 2 (28b)Lanes Under: 0  
(29)ADT: 50  
(30)Year of ADT: 2023  
(109)Truck ADT: 30%  
(19)Detour Length: 58 miles  
Speed Limit: 15 MPH

### APPRAISAL

(67)Structure Condition: 3 Intolerable - Correct  
(68)Deck Geometry: 2 Intolerable - Replace  
(69)Undrclear,Vert and Horiz: N Not applicable (NBI)  
(71)Waterway Adequacy: 6 Equal Minimum  
(72)Approach Alignment: 6 Equal Min Criteria  
(36)Traffic Safety Features:  
(a)Bridge Rail: 1 Meets Standards  
(b)Transition: 0 Substandard  
(c)Approach Rail: 0 Substandard  
(d)Approach Rail Ends: 0 Substandard  
(113)Scour Critical: 6 Calcs not made

### PROPOSED IMPROVEMENTS

(75a)Type of Work:  
(75b)Work Done By:  
(76)Length of Improvement: 47 ft  
(94)Bridge Improvement Cost: \$217,000  
(95)Rdwy Improvement Cost: \$22,000  
(96)Total Project Cost: \$326,000  
(97)Year of Cost Estimate: 2023  
(114)Future ADT: 75  
(115)Year of Future ADT: 2042  
YEAR PROGRAMMED:

### NAVIGATION DATA

(38)Navigation Control: Permit Not Required  
(39)Vertical Clearance:  
(40)Horizontal Clearance:  
(111)Pier Protection:  
(116)Lift Bridge Vert Clr:

### ENVIRONMENTAL

Environmental Concerns: No

### INSPECTION

(90)Inspection Date: 10/19/2023 (91)Inspection Frequency: 12 months  
(92)Supplemental Inspections Frequency: (93)Date of Inspections:  
(a)Fracture Critical Detail: NA (a)FC Inspection Date:  
(b)Underwater Inspection: NA (b)UW Inspection Date:  
(c)Fatigue Detail (OS) Inspection: NA (c)Fatigue Detail (OS) Date:  
(d)In-Depth Inspection: NA (d)In-Depth Date:  
(e)Confined Space Inspection: NA (e)Confined Space Date:  
Channel Cross Section Year: 2023  
Equipment Needed: None



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Xref Structure Name:		District:	02

### WEARING SURFACE and DEAD LOAD INFORMATION

Asphalt:	0.0 inches	Concrete:	0.0 inches
Granular:	1.5 inches	Timber:	0.0 inches

### POSTING INFORMATION

#### WEIGHT

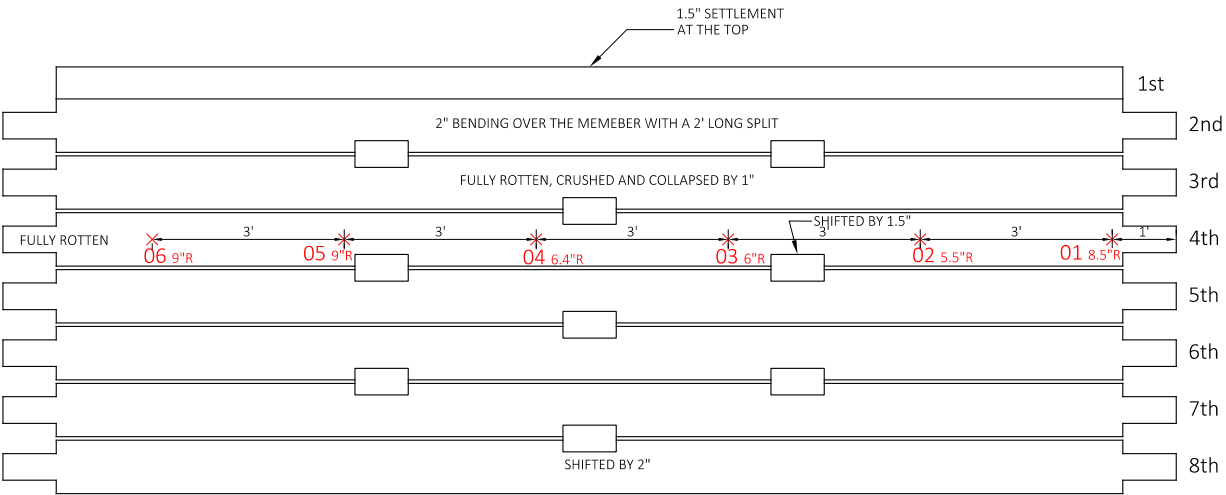
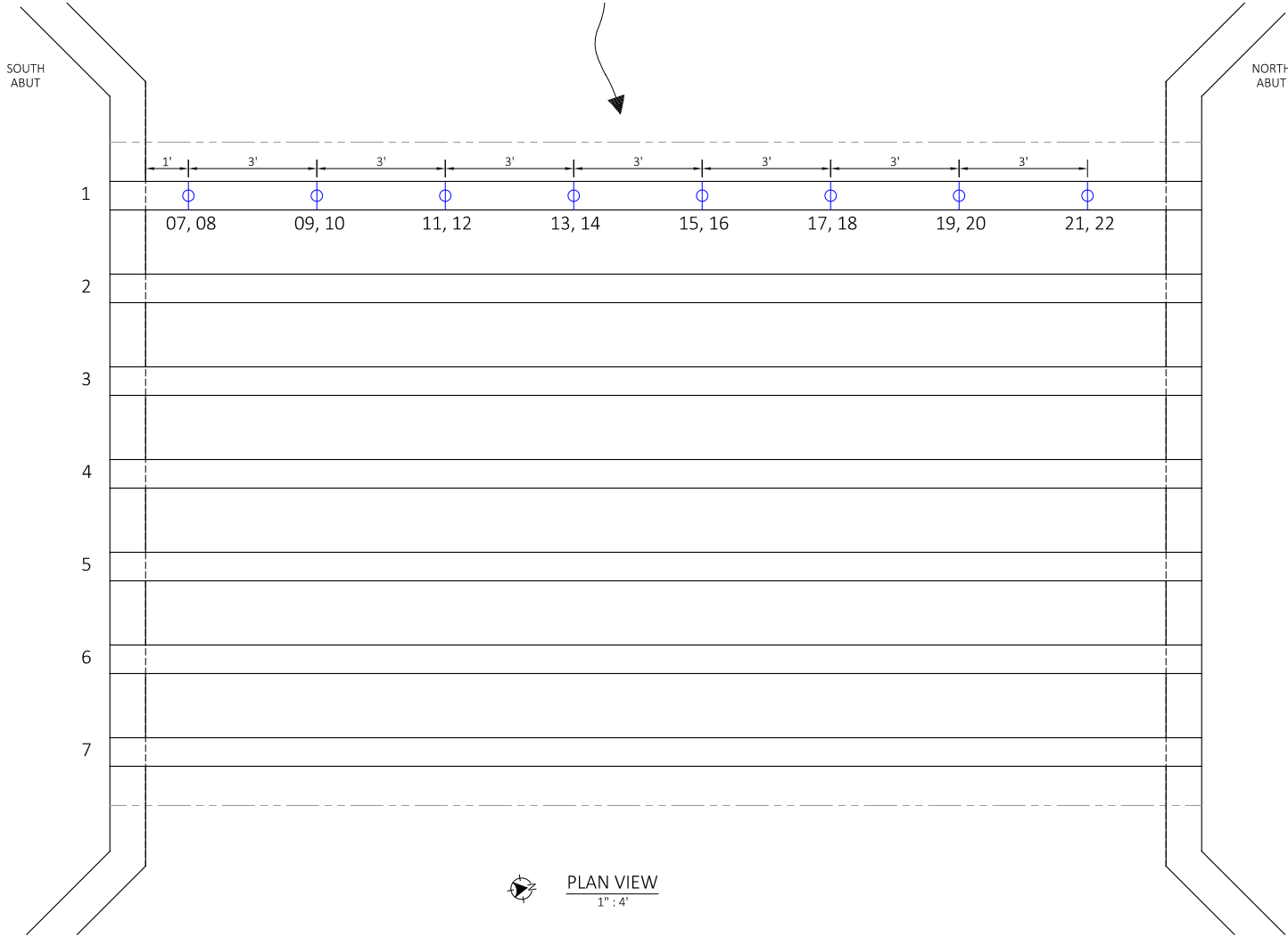
Load Analysis Date:		Bridge Factor:	
Load Analysis Required:	B Initial Analysis Req	Route Color:	
<b>Load Rating Analysis</b>		<b>Recommended</b>	<b>Actual</b>
<b>IR (tons)</b>	<b>OR (tons)</b>	<b>Posting(tons)</b>	<b>Posting(tons)</b>
H Truck			
HS Truck			
Type3	Type3	3	3
Type 3S2	Type 3S2	3	3
Type 3-3	Type 3-3	3	3
	Axle Limit		

#### HEIGHT

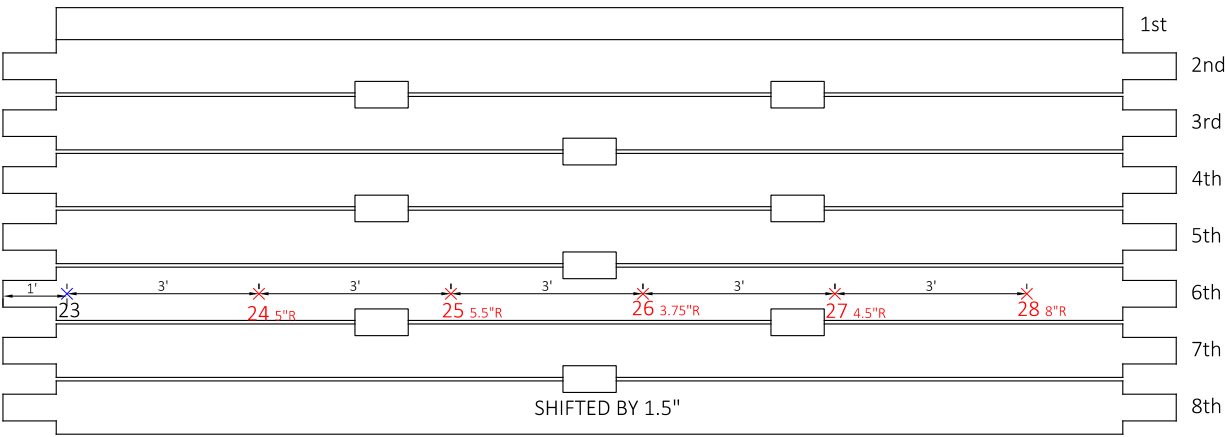
	<b>Recommended</b>	<b>Actual</b>
Height Posting:		

#### ACTUAL WIDTH POSTING

Single Lane All Vehicles:	N
Single Lane Trucks/Buses:	N



SOUTH ABUTMENT  
1" : 3'



NORTH ABUTMENT  
1" : 3'

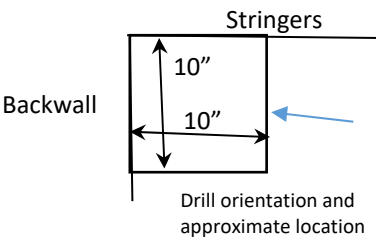
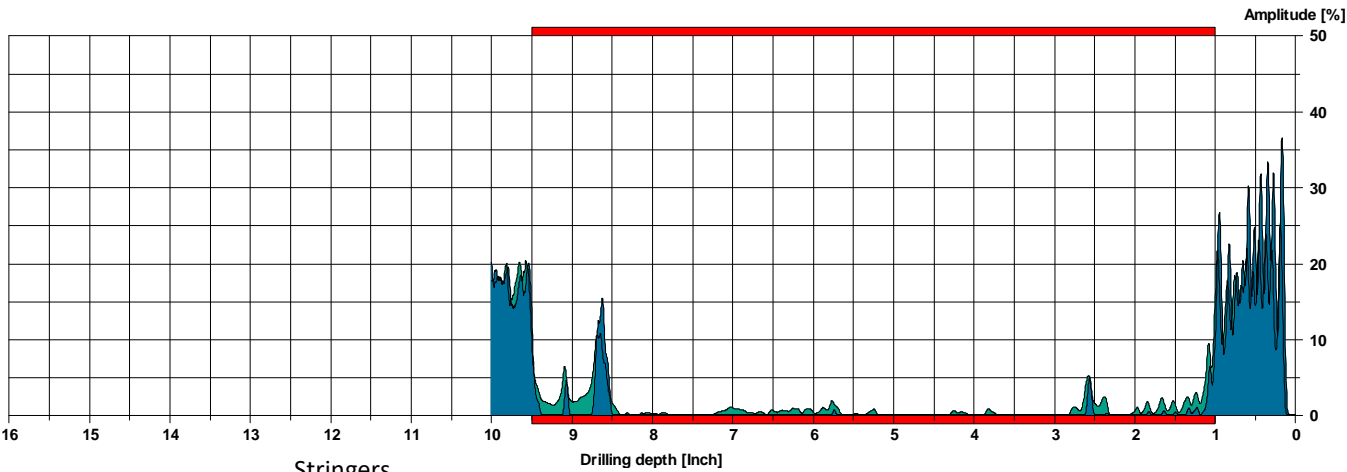
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

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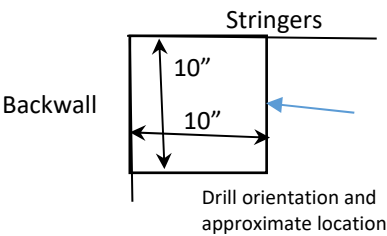
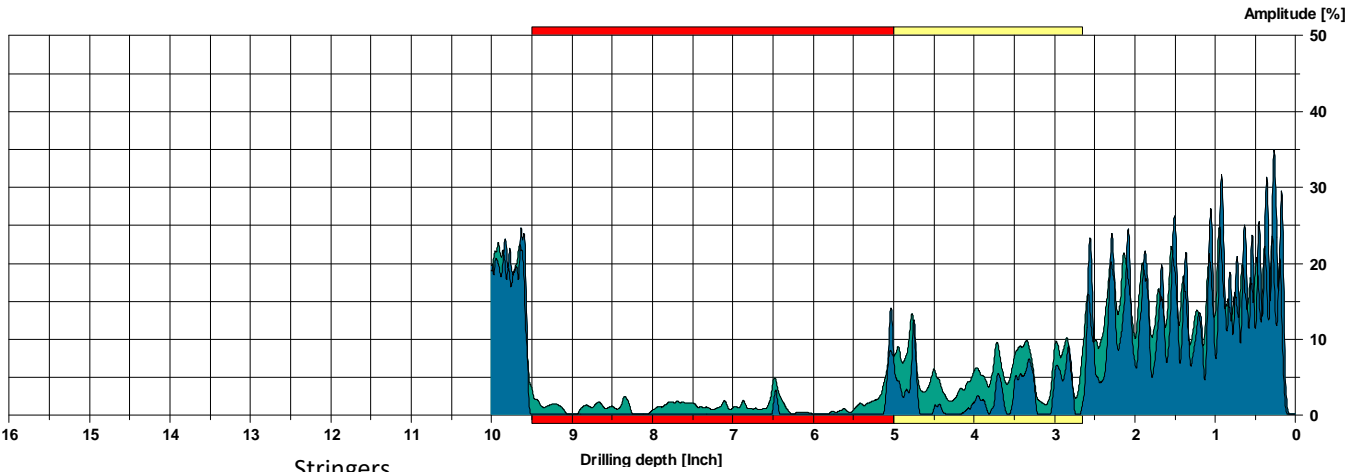
10/19/2023

Drill: E-1023-01



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 1' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
8.5" of rot detected from 1" to 9.5".

Drill: E-1023-02



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 4' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
5.5" of rot detected with soft wood from 2.6" to 5".



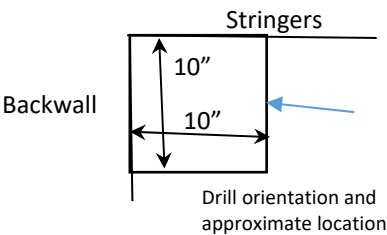
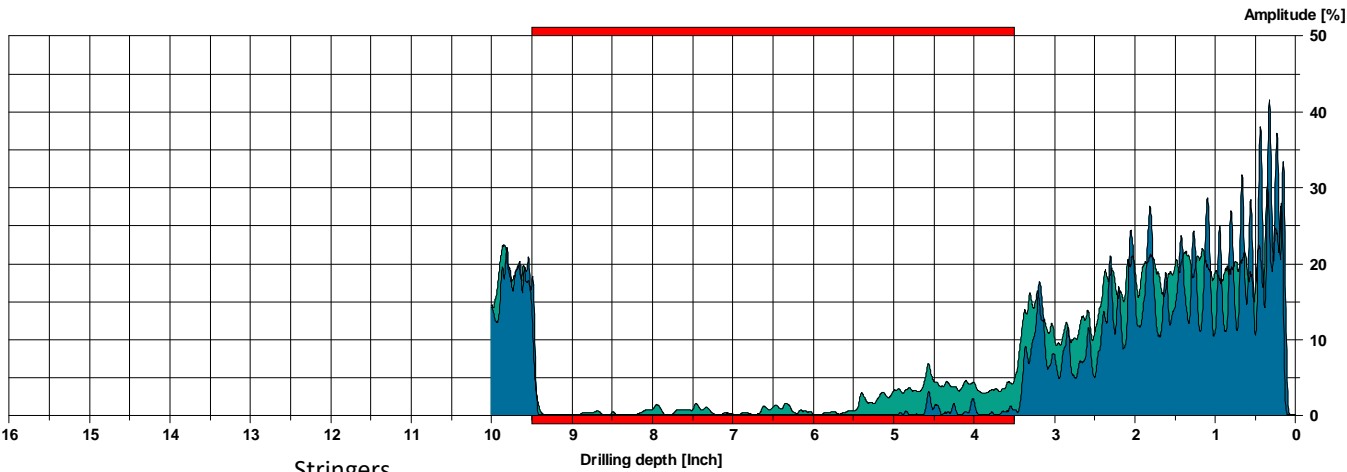
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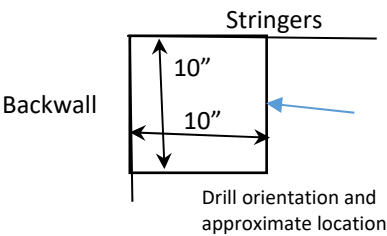
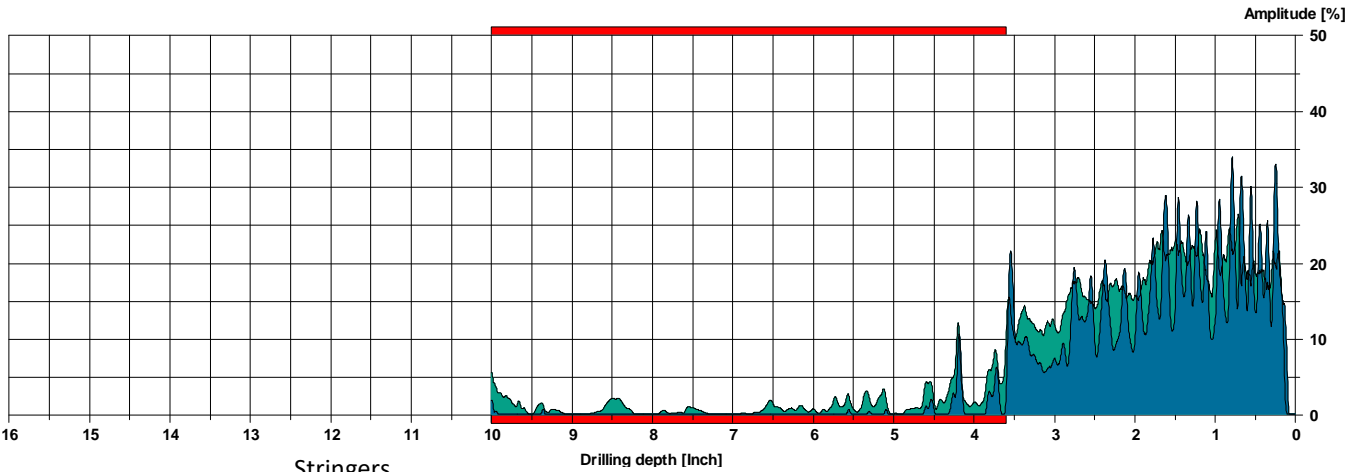
10/19/2023

Drill: E-1023-03



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 7' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
6" of rot detected from 3.5" to 9.5".

Drill: E-1023-04



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 10' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
6.4" of rot detected from 3.6" to 10".

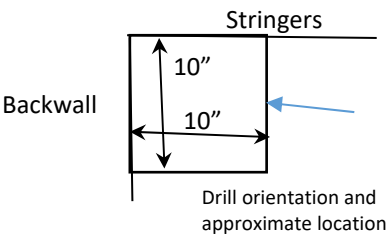
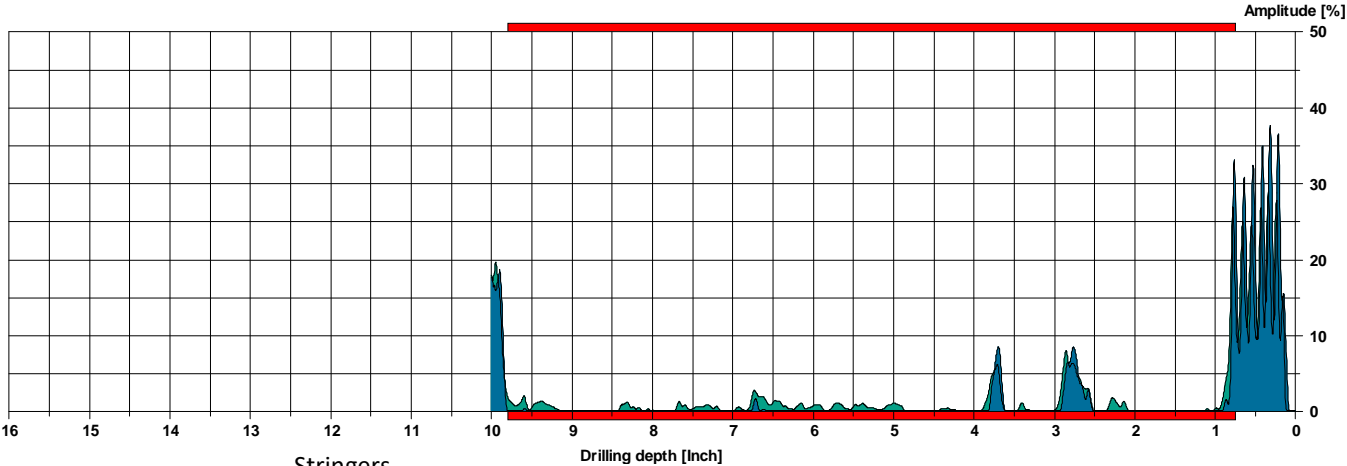
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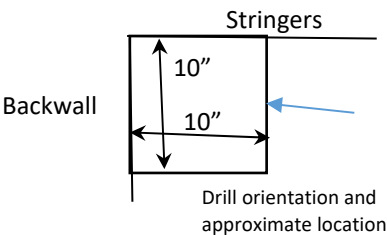
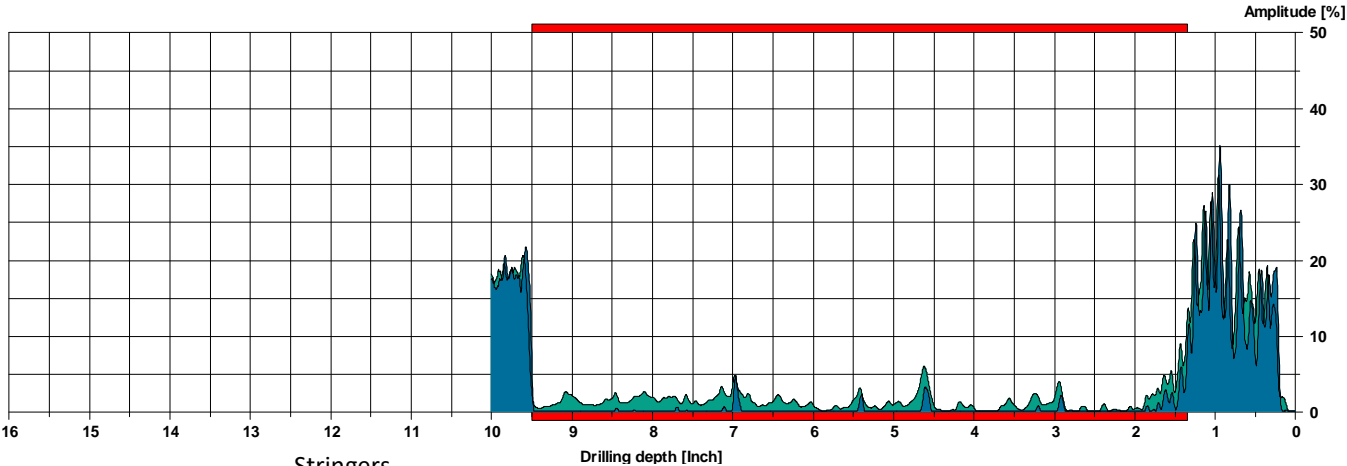
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Drill: E-1023-05



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 13' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
9" of rot detected from 0.75" to 9.8".

Drill: E-1023-06



**Unit:** South Abutment      **Member:** Fourth timber from the top  
**Location:** 16' from southwest corner of abutment.  
**Inspector Interpreted Profile:**  
9" of rot detected from 1.35" to 9.5".

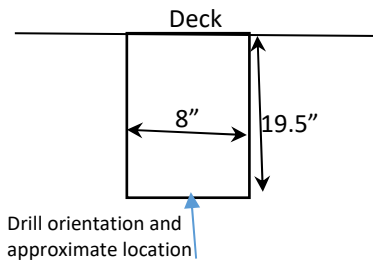
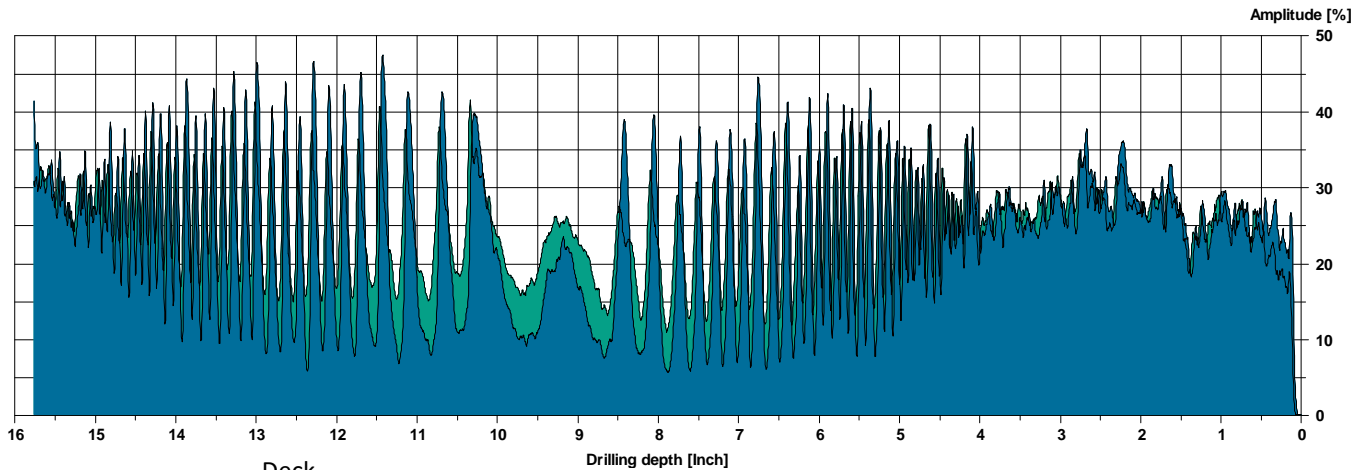
MICRO-RESISTANCE DRILLING LOG

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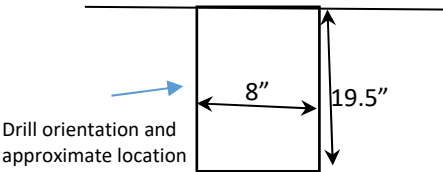
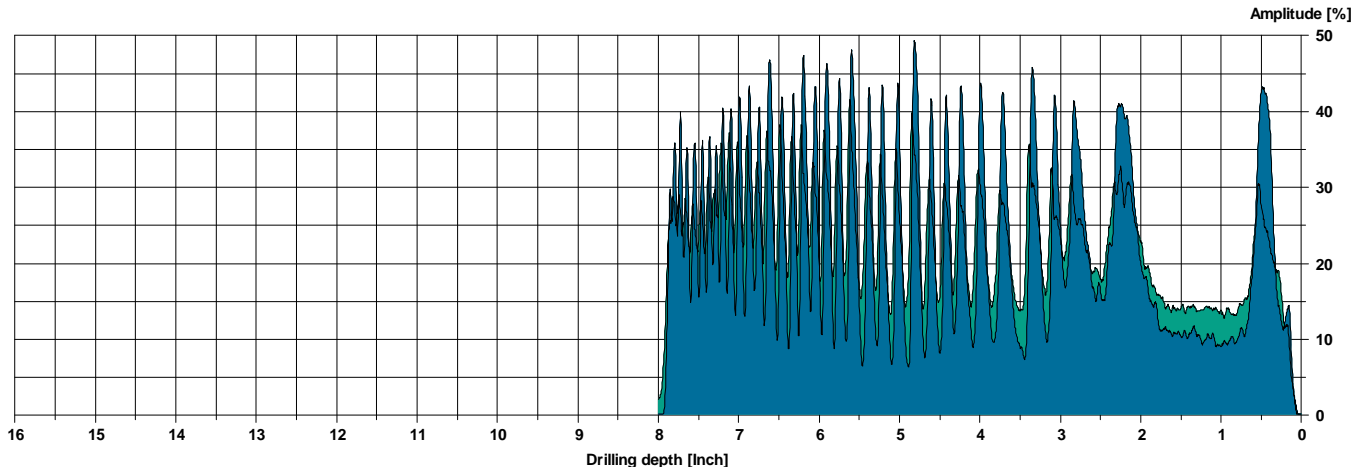
10/19/2023

Drill: E-1023-07



Span: 1    Member: Beam 1  
Location: 1' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

Drill: E-1023-08



Span: 1    Member: Beam 1  
Location: 1' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

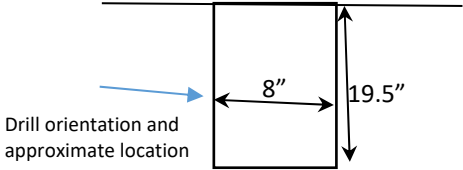
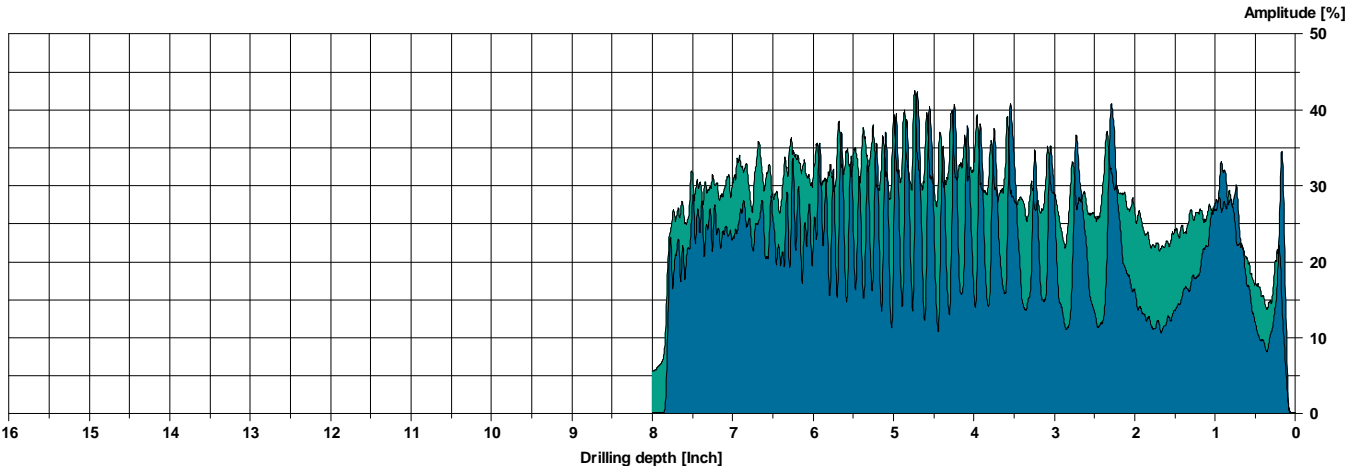
MICRO-RESISTANCE DRILLING LOG

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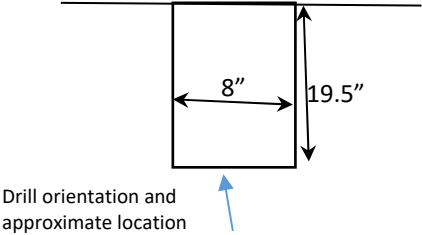
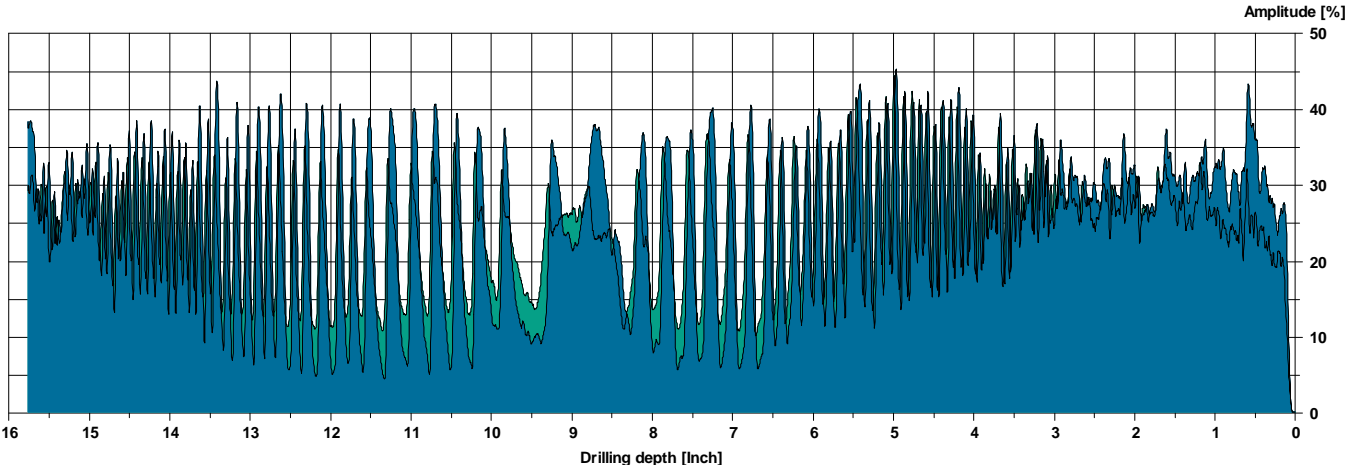
10/19/2023

Drill: E-1023-09



**Span: 1    Member: Beam 1**  
**Location: 4' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

Drill: E-1023-10



**Span: 1    Member: Beam 1**  
**Location: 4' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

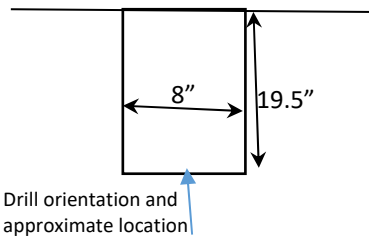
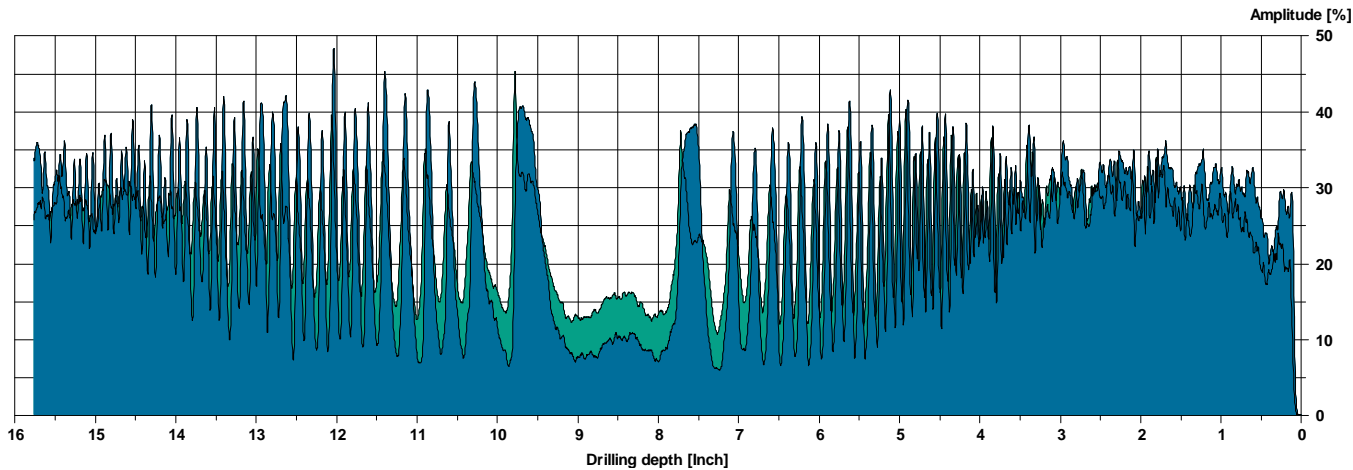
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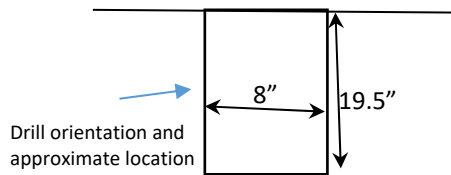
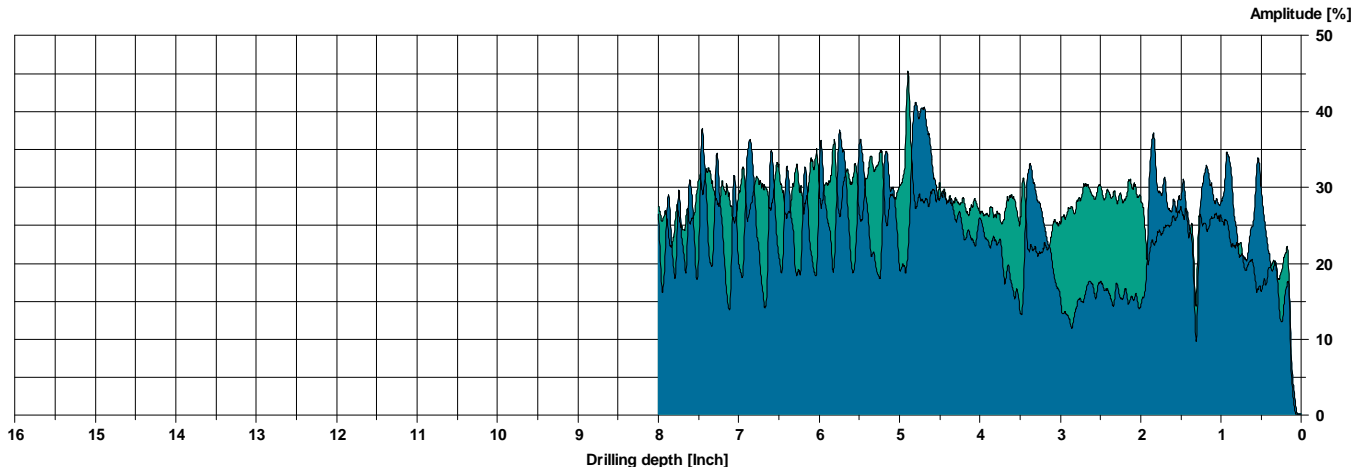
10/19/2023

Drill: E-1023-11



Span: 1 Member: Beam 1  
Location: 7' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

Drill: E-1023-12



Span: 1 Member: Beam 1  
Location: 7' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

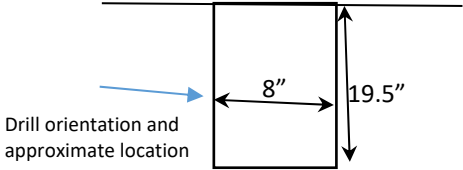
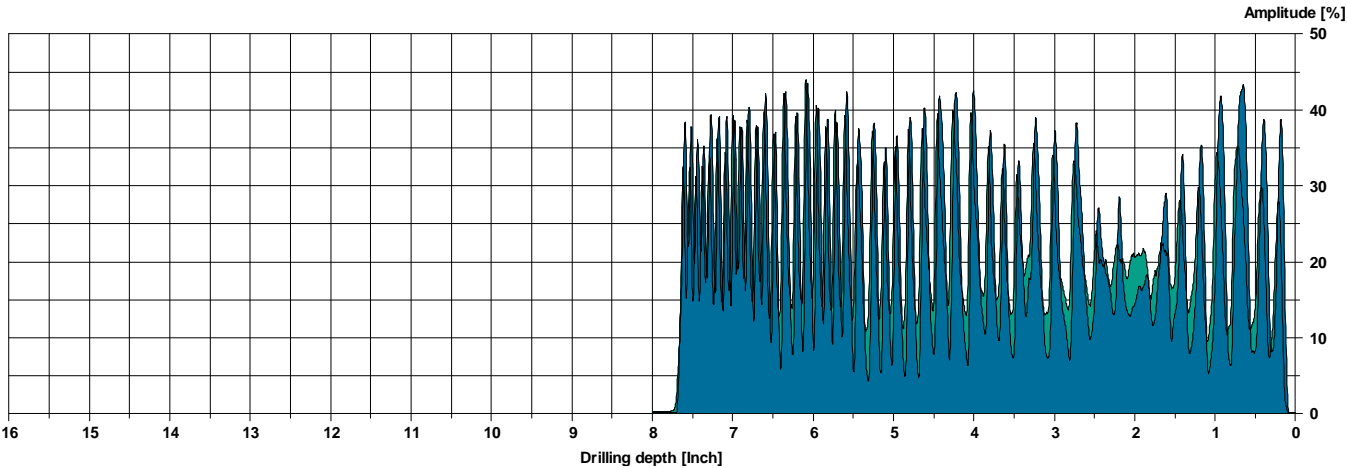
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IML-RESI Power Drill PD400

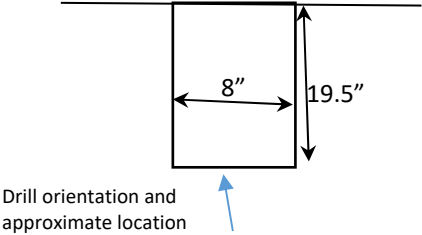
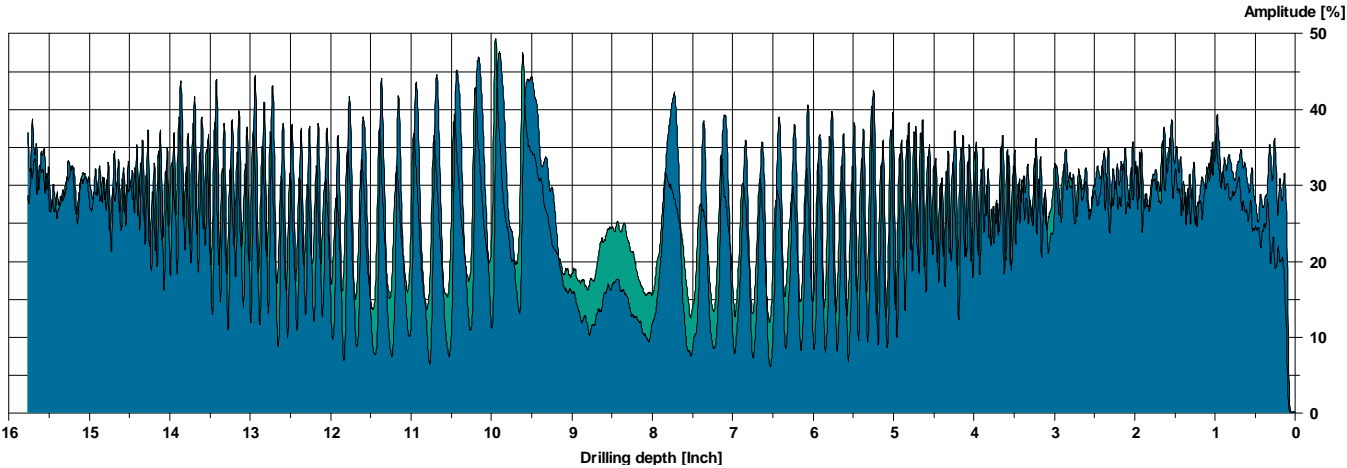
10/19/2023

Drill: E-1023-13



**Span: 1    Member: Beam 1**  
**Location: 10' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

Drill: E-1023-14



**Span: 1    Member: Beam 1**  
**Location: 10' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

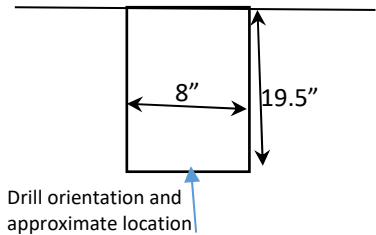
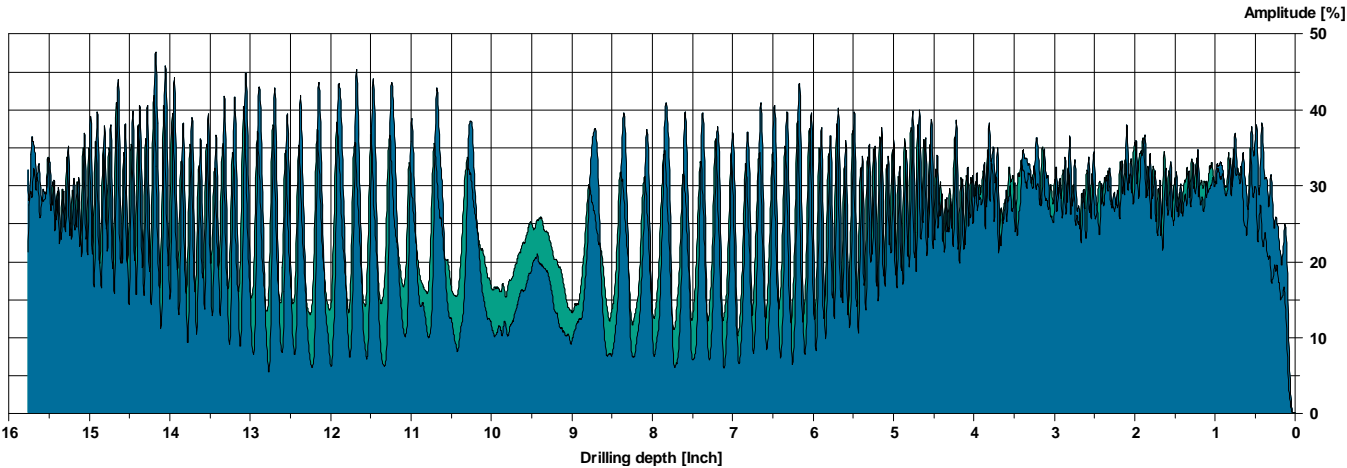
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

IML-RESI Power Drill PD400

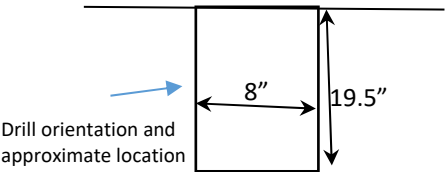
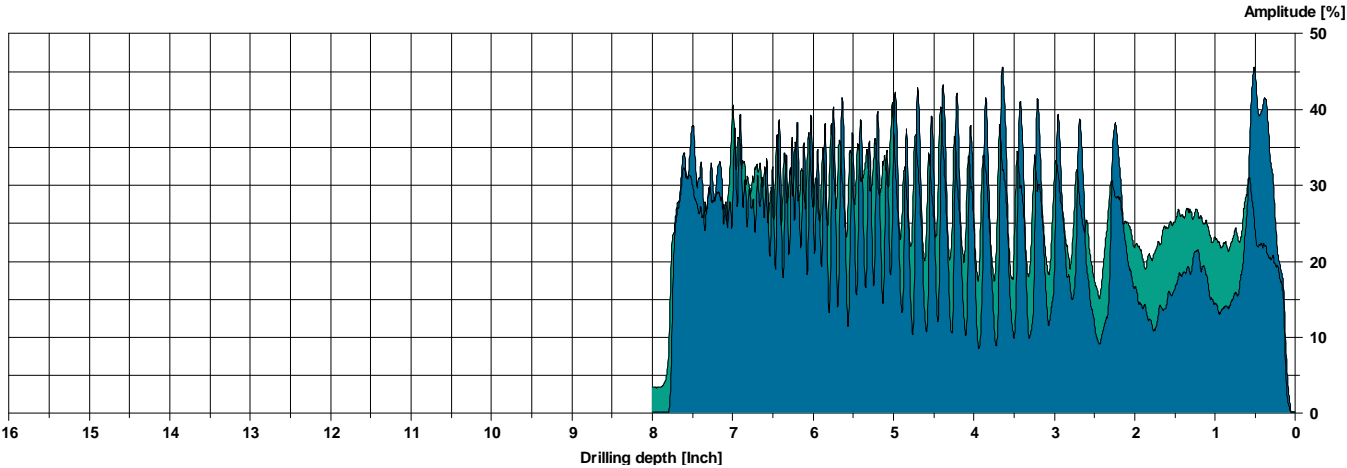
10/19/2023

Drill: E-1023-15



Span: 1    Member: Beam 1  
Location: 13' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

Drill: E-1023-16



Span: 1    Member: Beam 1  
Location: 13' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

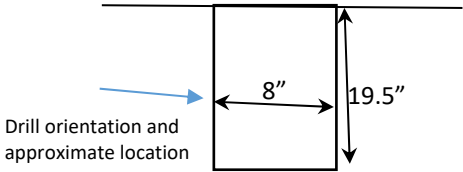
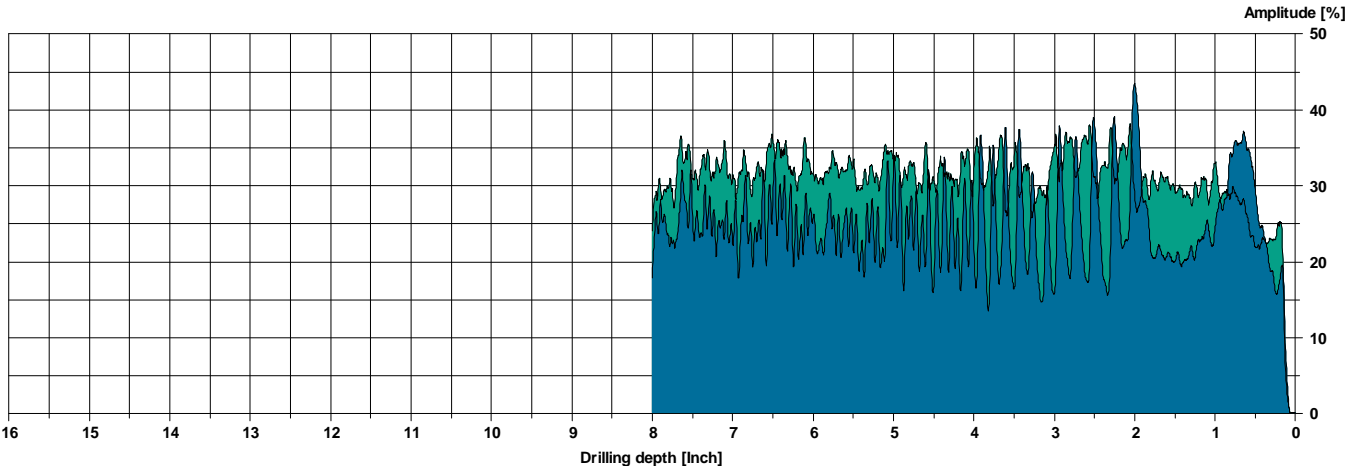
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

IML-RESI Power Drill PD400

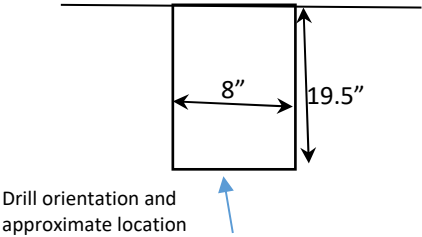
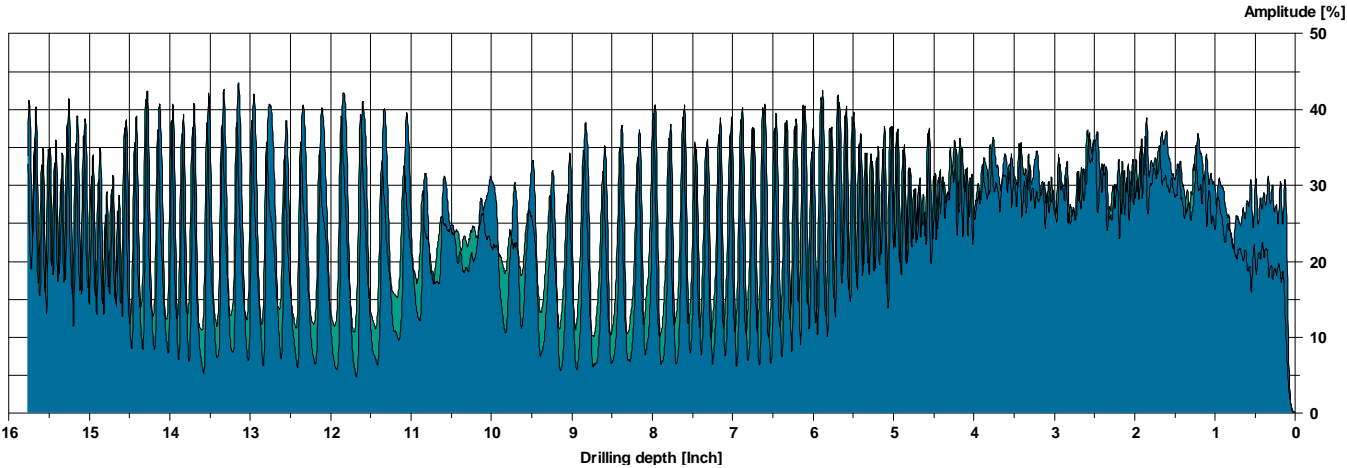
10/19/2023

Drill: E-1023-17



Span: 1 Member: Beam 1  
Location: 16' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

Drill: E-1023-18



Span: 1 Member: Beam 1  
Location: 16' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.



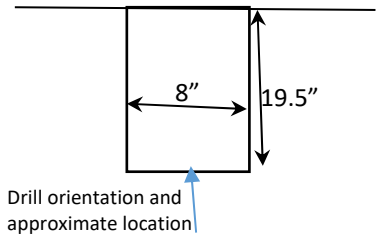
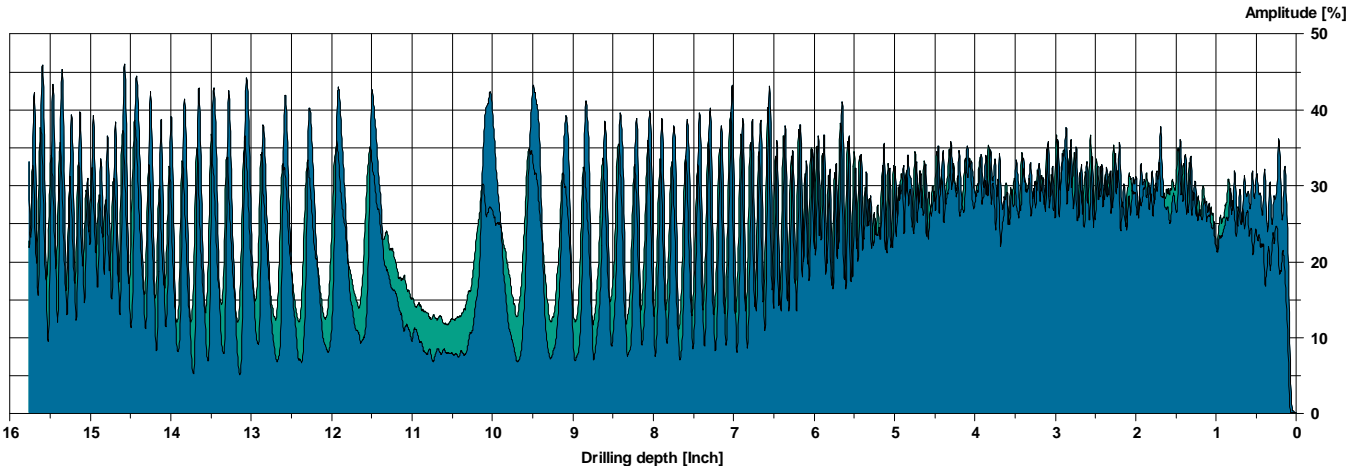
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

IML-RESI Power Drill PD400

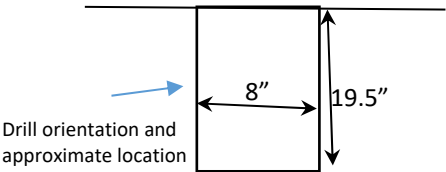
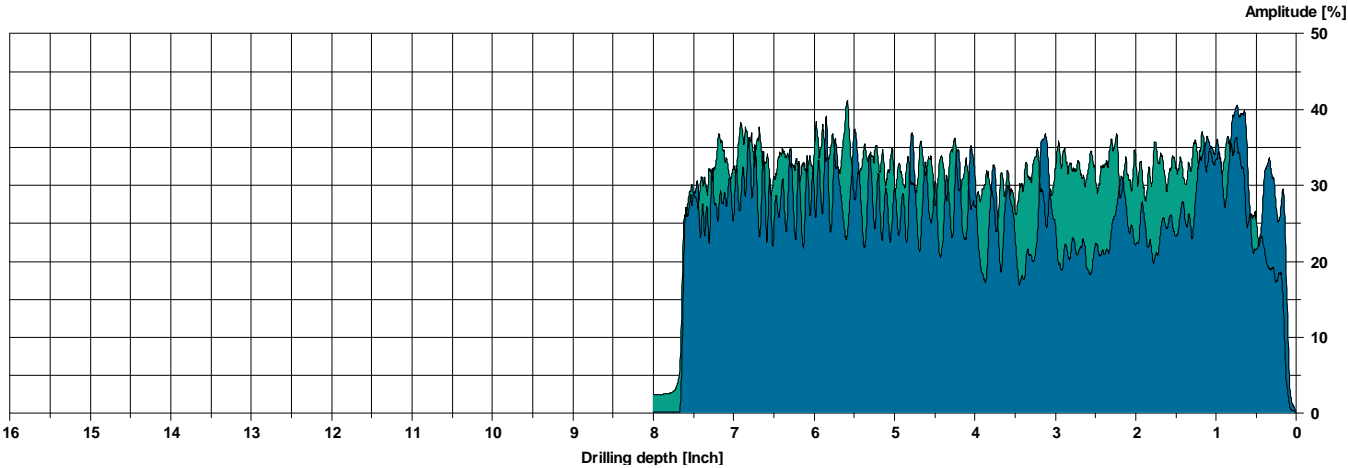
10/19/2023

Drill: E-1023-19



Span: 1    Member: Beam 1  
Location: 19' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

Drill: E-1023-20



Span: 1    Member: Beam 1  
Location: 19' from the south abutment.  
Inspector Interpreted Profile:  
No rot detected.

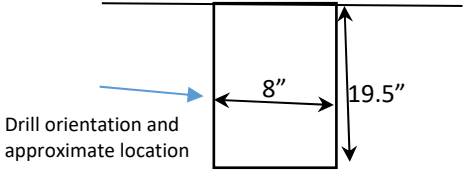
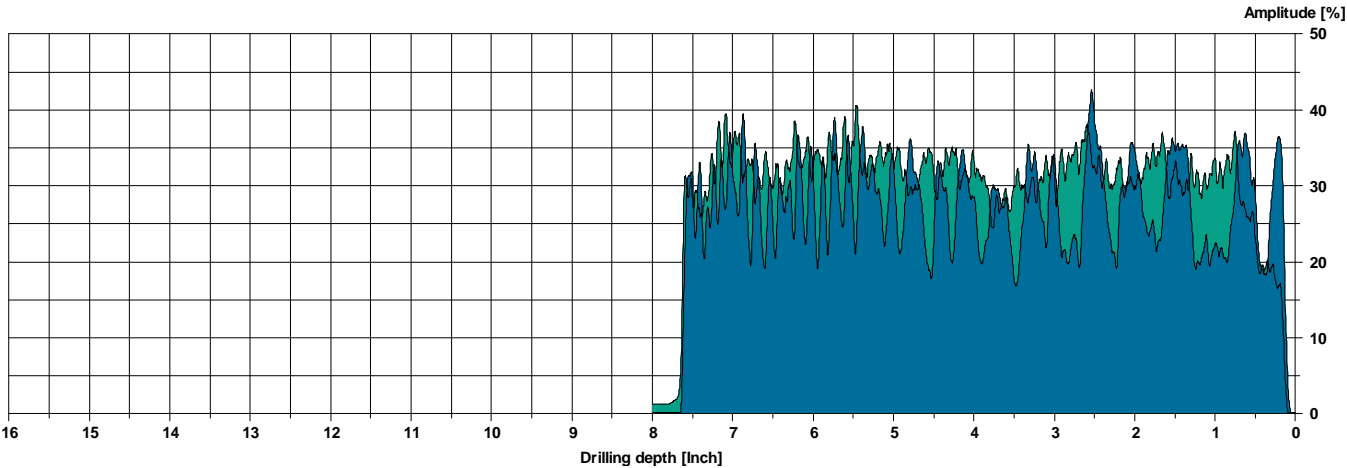
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

IML-RESI Power Drill PD400

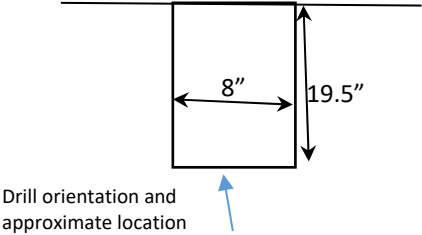
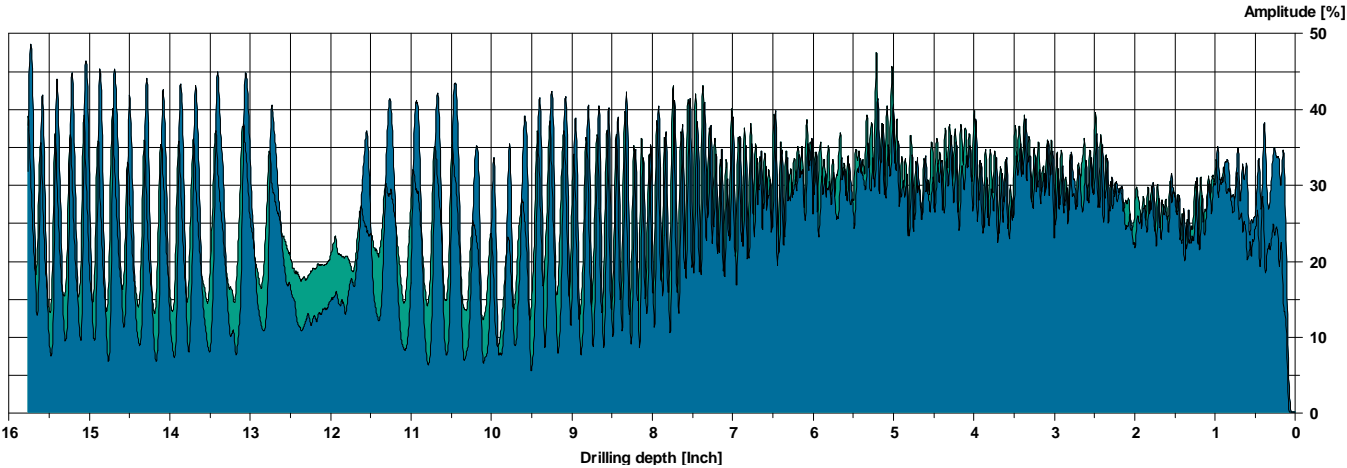
10/19/2023

Drill: E-1023-21



**Span: 1    Member: Beam 1**  
**Location: 22' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

Drill: E-1023-22



**Span: 1    Member: Beam 1**  
**Location: 22' from the south abutment.**  
**Inspector Interpreted Profile:**  
No rot detected.

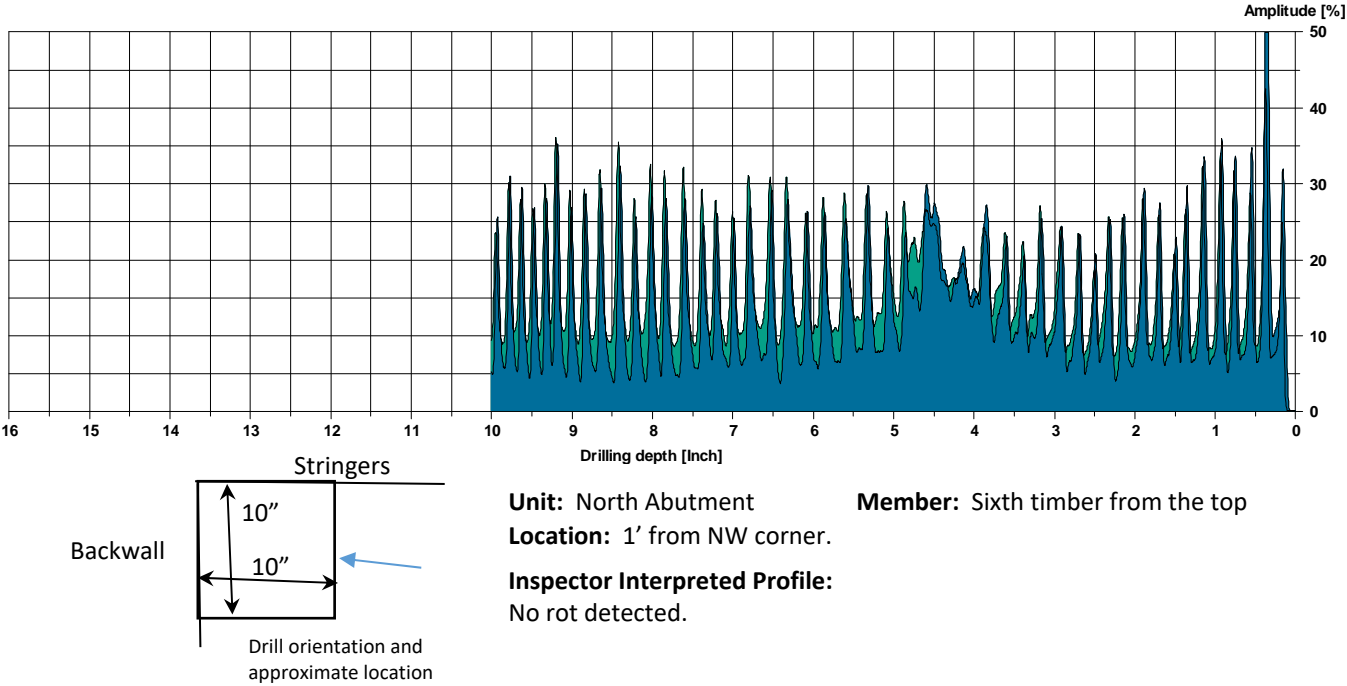
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

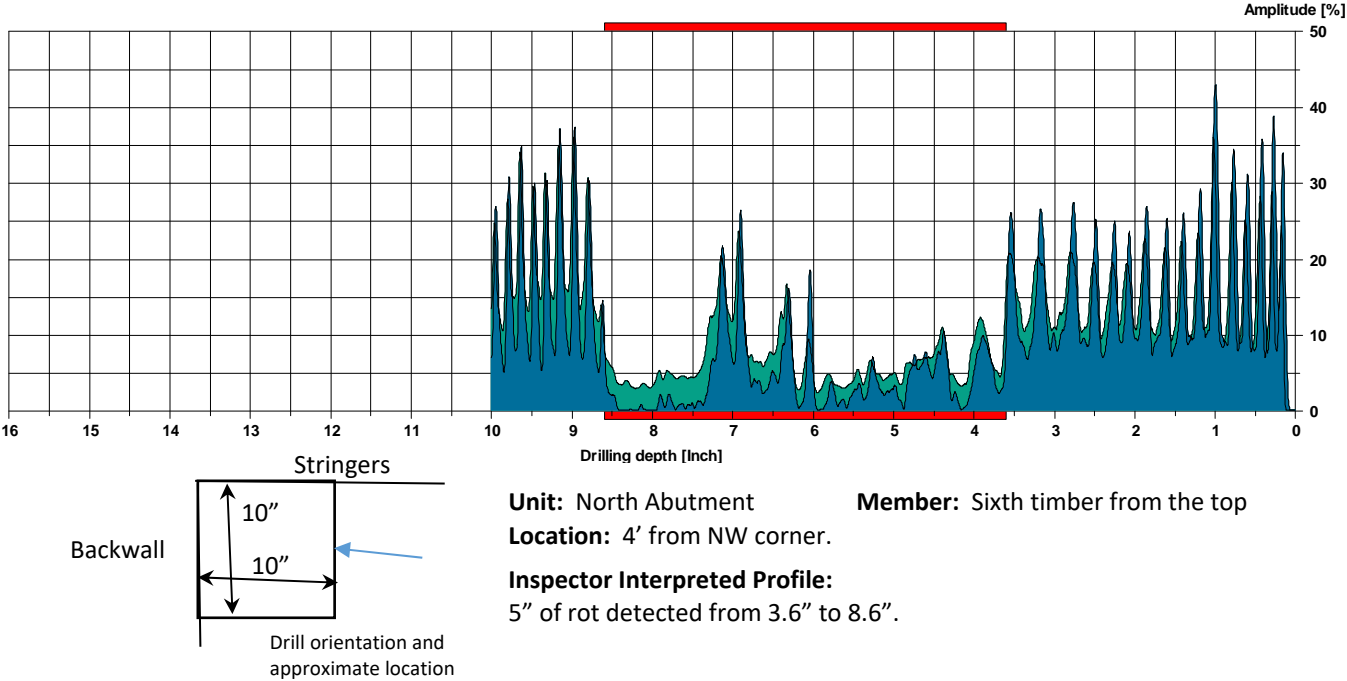
IML-RESI Power Drill PD400

10/19/2023

Drill: E-1023-23



Drill: E-1023-24



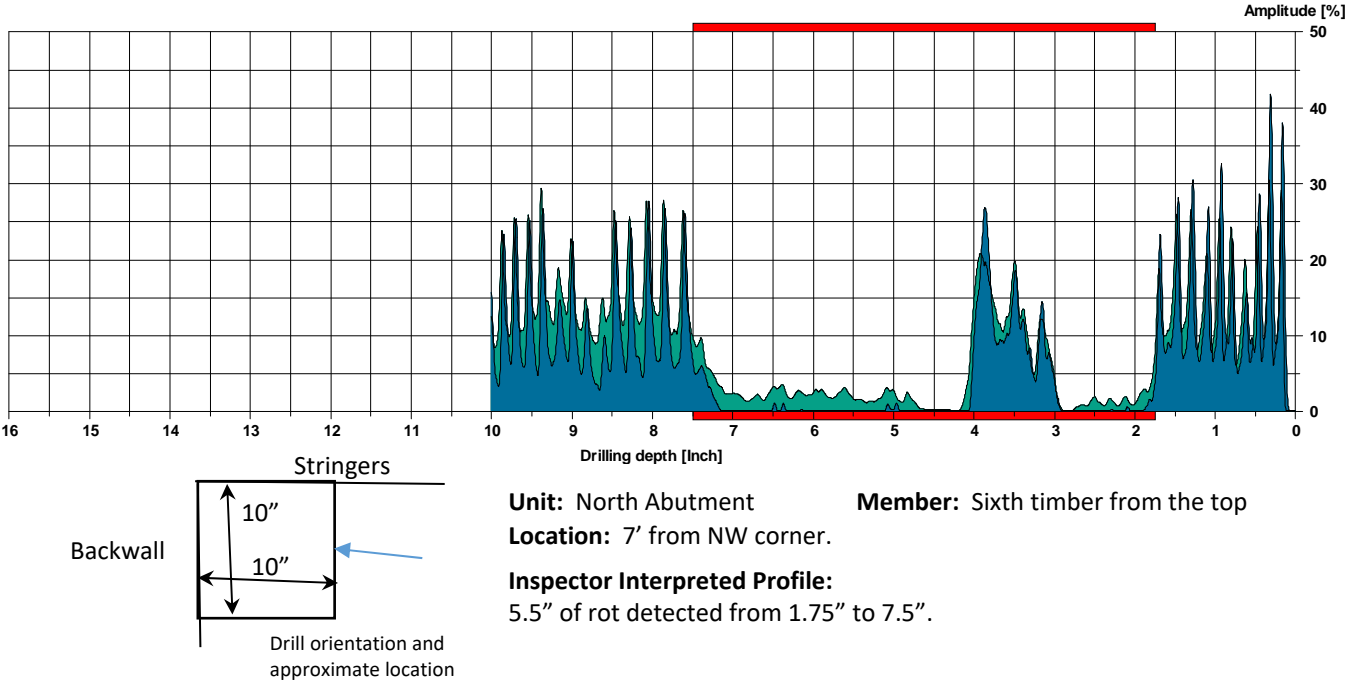
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

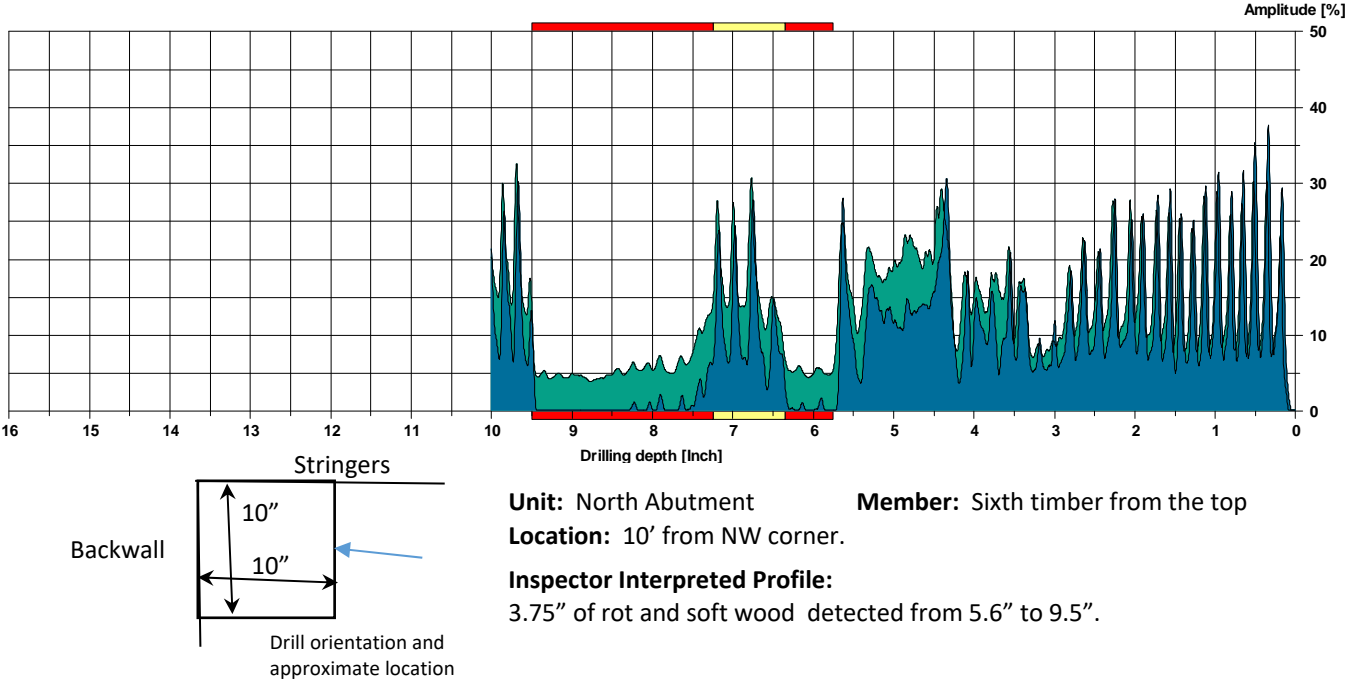
IML-RESI Power Drill PD400

10/19/2023

Drill: E-1023-25



Drill: E-1023-26



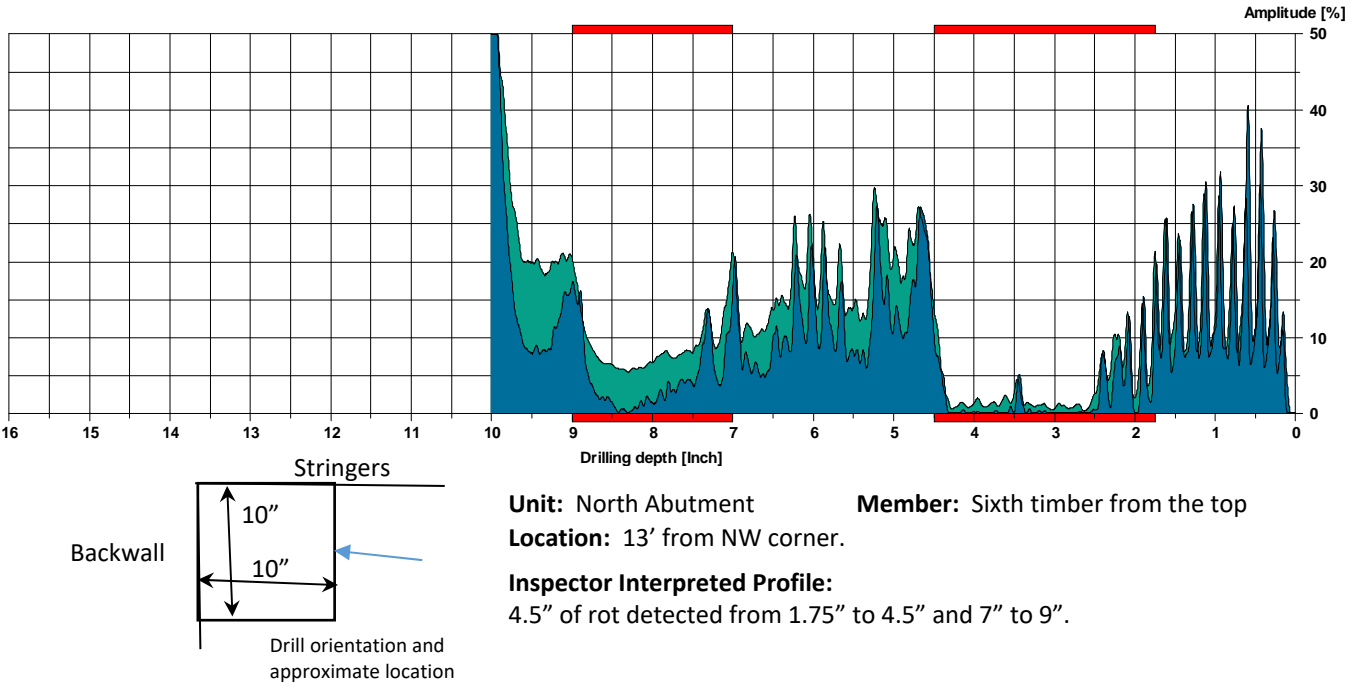
MICRO-RESISTANCE DRILLING LOG

Bridge 29263

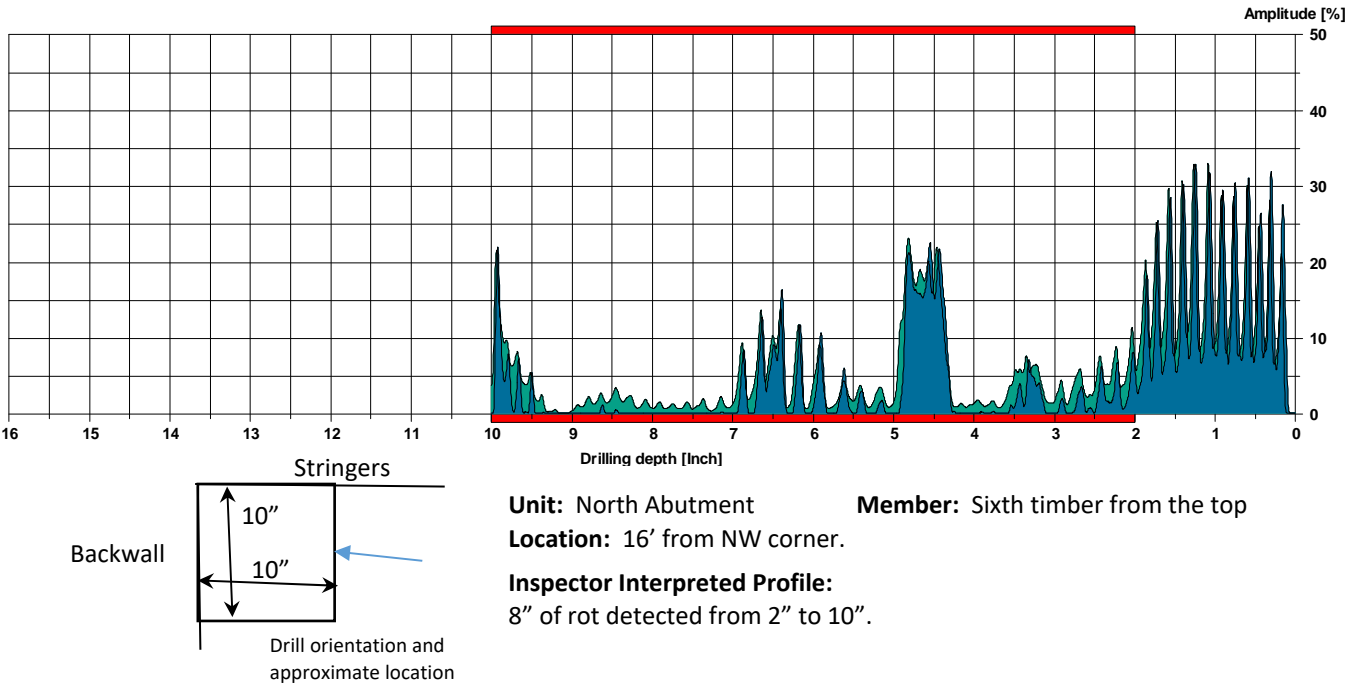
IML-RESI Power Drill PD400

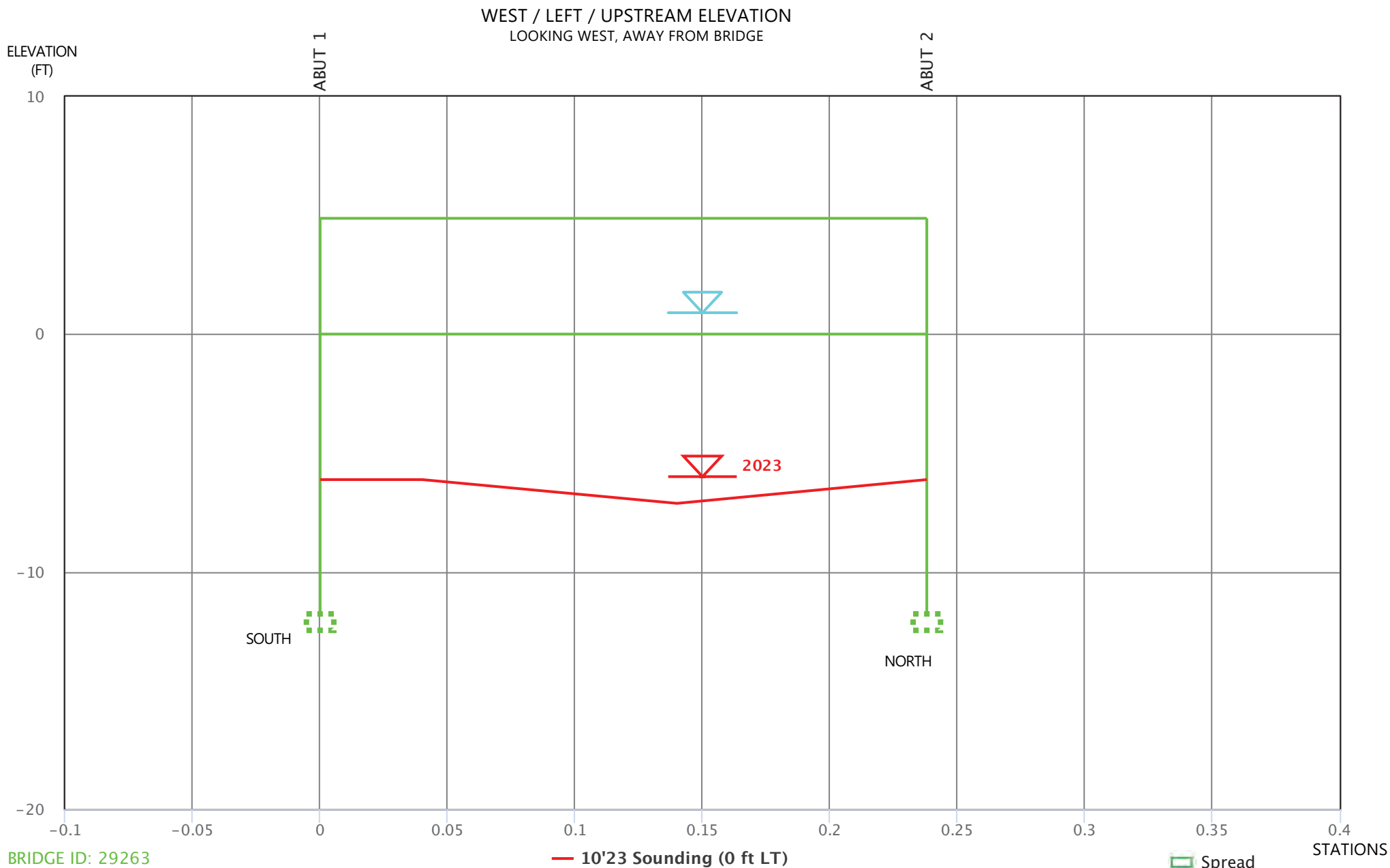
10/19/2023

Drill: E-1023-27



Drill: E-1023-28





UPPER CROOKED RIVER  
UNKNOWN (P) COUNTY  
LEFT SIDE OF THE BRIDGE – UPSTREAM  
DATA SOURCE: Field Measurements  
DRAW STATIONS FROM: 0 ft TO: 23.8 ft  
STATION SCALE 10 ft:1 in ELEVATION SCALE 10 ft:1 in



## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



South approach.



West elevation. The girders are numbered from west to east.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



West / upstream channel.



East / downstream channel.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



West / upstream channel.



East / downstream channel.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



North approach: the old [WEIGHT LIMIT 12 TONS] sign which was later replaced.



South approach: the old [WEIGHT LIMIT 12 TONS] sign which was later replaced.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
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Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



11/2/2023: North approach new [WEIGHT LIMIT 12 TONS] sign.



11/2/2023: South approach new [WEIGHT LIMIT 12 TONS] sign.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
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Xref Structure Name:		District:	02



Soffit, superstructure, south abutment, and channel. The abutment has rot and shifting; the top has 1.5-inch settlement.



(CS4) South abutment: the third and fourth timbers from the top are fully rotten; the third timber has crushing.



## Idaho Transportation Department Bridge Inspection Report

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Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
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(CS4) South abutment: the 4th beam from the top has a 1.5-inch offset.



(CS4) South abutment: the bottom timber has a 2-inch offset.





## Idaho Transportation Department Bridge Inspection Report

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Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



(CS4) SE wingwall: the timber is heavily burnt.



SW wingwall: (CS4) the timber is heavily burnt and is (CS3) leaning 5-degrees.





## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
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Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



(CS4) NW wingwall: the timber is heavily burnt.



(CS4) NE wingwall: the timber is heavily burnt.



## Idaho Transportation Department Bridge Inspection Report

Bridge Key:	29263	Structure Name:	X994250 16.15
(6)Features Intersected:	UPPER CROOKED RIVER	(9)Location:	30.2 E 12.6 S GRANGEVILLE
Facility Carried(Route):	CROOKED RIVER ROAD	Admin Jurisdiction:	4900 Idaho County
Xref Structure Name:		District:	02



(CS4) North abutment: the bottom timber has a 1.5-inch offset.